

Premium Industrial Park Phase 2

Comprehensive Development Review Addendum

1 INTRODUCTION AND BACKGROUND

A Comprehensive Development Review was presented to the RM Council in August 2016 to support the zoning of SE 15-38-5-W3M to a heavy industrial district and the initial phase of subdivision. Following a meeting with members of the RM Administration, the owner was informed of the RM's preference to see the balance of the industrial park subdivided in a single phase rather than in multiple phases as represented in previous submissions.

Since the original approval of the initial phase of subdivision, the RM has adopted a new Official Community Plan and Zoning Bylaw that presides over land use and development within the subject lands. Additionally, the North Concept Plan is pending approval and offers additional information regarding the form and configuration of development and plans for the construction of regional scale infrastructure in the vicinity of the subject lands.

The above-noted changes combined with changes in the industrial market have contributed to the owner proceeding with a revised plan of subdivision that deviates from the plan represented in the original CDR report. The original CDR represented the intention to subdivide 22 industrial lots. The current plan of subdivision proposes to increase the number of lots to 34.

This report has been prepared to demonstrate how the current plan of subdivision relates to the current presiding policies and regulations.

2 POLICY COMPLIANCE

2.1 P4G OFFICIAL COMMUNITY PLAN

Land use and development within the subject lands are regulated by the P4G Official Community Plan (DOCP) and Zoning Bylaw. The following section is intended to speak to how the current plan of subdivision aligns with the presiding DOCP policies.

Section 14.0 Rural Industrial

14.3.1 Designation on Schedule B – District Land Use Map

Areas designated as Rural Commercial/Industrial on Schedule B – District Land Use Map shall be further designated as Rural Industrial areas or Rural Commercial areas through more detailed planning. Concept Plans and other detailed planning shall identify the type(s) of Rural Industrial land use(s) being proposed.

The North Concept Plan refines the designation of the subject lands as a combination of Rural Light Industrial and Green Network Study.

14.3.2 Rural Industrial Development

Rural Industrial development shall be located in areas designated as Rural Commercial/Industrial on Schedule B – District Land Use Map, and in areas that have been identified as suitable for Rural Industrial development through the detailed planning referenced in section 14.3.1.

The subject lands are appropriately designated on the District Land Use Map for the development proposed.

14.3.3 Location Criteria

Rural Industrial developments must be located where:

- a) *The carrying capacity of the lands proposed for development and the surrounding area based on site conditions, environmental considerations, P4G District Official Community Plan 33 potential impacts, and other factors that may warrant consideration in the design of the proposal are addressed;*
- b) *Impacts on regional drainage patterns and other regional ecological systems are minimized;*
- c) *The suitability and availability of municipal and other services and infrastructure necessary to support the proposal are considered;*
- d) *The design is compatible with that of the surrounding area;*
- e) *Existing roads and infrastructure are sufficient to support the development while impacts to existing roadways and additional costs of maintenance are minimized;*
- f) *Nodal development is planned where key intersections of provincial highways, municipal roadways, and the Saskatoon Freeway can support access;*
- g) *Lands are not prone to natural hazards;*
- h) *Lands do not have unique historical or archaeological features;*
- i) *Lands do not have significant wildlife habitat;*
- j) *Lands do not have high quality recreational resources;*
- k) *Surface and groundwater resources will not be impacted; and*
- l) *Any other costs to Corman Park associated with the development are minimized.*

The current subdivision application is accompanied by a sealed set of construction drawings detailing the intended method and standard for the construction of a new internal roadway, storm, grading and water distribution services within the property. These details have been shared with the Water Security Agency and confirmed as acceptable as proposed relative to current provincial regulations and standards.

The previously approved CDR report speaks to the balance of the items in this list Council's approval of a light industrial zoning reinforces the municipality's support for the intended land use at this location. The previous phase of subdivision established the full extent of drainage works required to support development within the subject property and provided for the full dedication of land required to serve the lands. The detailed design drawings submitted to support the subdivision confirm that the minor alteration in the lot sizing and boundaries has no impact on the design and construction of the drainage works.

14.3.4 Industrial Parks

Industrial uses shall be restricted to industrial parks unless it can be clearly demonstrated that an industrial use has specific location requirements that limit its location to a specific alternate site.

The plan of subdivision represents the intention to subdivide multiple lots within the subject lands.

14.3.6 Buffered Uses in Industrial Parks

Industrial parks with a high potential for land use conflicts must include buffering from non-industrial uses of land, including locating uses with fewer impacts near roadways, and other criteria as set out in this Plan and the District Zoning Bylaw.

Light Industrial zoning is considered to include medium-scale industries which may have periodic emissions that extend beyond the site boundaries and may on occasion extend beyond typical daytime business hours. Offsite nuisance-related impacts are mitigated by the predominant industrial use of neighbouring properties combined with the inclusion of a 30 m wide municipal utility corridor extending around the perimeter of the development.

14.3.7 Hazardous Industries

Industries using hazardous materials or producing waste dangerous goods as defined by The Environmental Management and Protection Act, 2010 may be considered subject to providing evidence of compliance with the National Fire Code of Canada, The Hazardous Substances and Waste Dangerous Goods Regulations, and any other applicable codes, standards and provincial and federal approvals.

The range of uses capable of being supported within the proposed subdivision is regulated by the provisions of the Light Industrial Zoning standards as defined by the P4G Zoning Bylaw.

Section 17.0 Green Network Study Area

17.3.1 Refinement

The P4G municipalities will work collaboratively to refine the boundaries of the Green Network Study Area.

A significant portion of the subject lands is designated as Green Network Study Area. Lands with this designation are intended to be used to address regional stormwater issues and where appropriate to support multifunctional green spaces that deliver environmental, cultural and recreational benefits. The plan for development for this area was previously approved in 2016 through the approval of the industrial zoning for this site. The current subdivision application represents the implementation of the approved concept represented in the 2016 CDR report.

17.3.2 Refinement Criteria

Refinement of the boundaries of the Green Network Study Area will be conducted according to a standard, consistent methodology that considers:

- a) Wetlands;*
- b) Drainage areas;*
- c) Conveyance areas, such as streams, rivers, and swales;*
- d) Habitat corridors for wildlife movement;*
- e) Important viewsheds, such as the viewshed of Wanuskewin Heritage Park; and*
- f) Important ecological areas.*

The previously approved CDR report submitted to support the current industrial zoning for the subject lands confirmed that the lands are not considered ecologically sensitive. The site had been previously excavated for gravel leaving the entire site in a disturbed state. A substantial area has been previously dedicated within the subject lands to account for the storage and management of stormwater. The stormwater management design accounts for the predevelopment natural storage provided by the lands as well as the incremental increase in run-off generated by hard-surface development within the subject lands leaving the cumulative function of the property for management of regional drainage unchanged but refined in regards to the location of this storage within the site.

Given the previous approval of the development concept represented by the 2016 CDR report and the subsequent approval of an application to rezone the subject lands to a light industrial district, we assume that the RM will support the current subdivision plan as a refinement to the Green Network Study designation.

2.2 NORTH CONCEPT PLAN

The subject lands are situated within the boundaries of the North Concept Plan (NCP) which was prepared to provide a land use, transportation, and servicing plan to guide the development of 6,920 hectares of land situated within the Rural Municipality of Corman Park No. 344 (Corman Park, the R.M.) along the northern boundary of the City of Saskatoon (COS) and the southern boundaries of the cities of Martensville and Warman.

The subject lands are included in Planning Cell 3 as described by the NCP. According to the NCP, lands within Planning Cell 3 are well suited to host the expansion of rural and urban industrial development. The design of this area supports heavy industrial development by providing access to rail, highways, and services, compatibility with adjacent land uses, and a location with surrounding transition buffers to minimize nuisance and risk. The NCP states:

With the proximity to existing and planned rural industrial developments in the vicinity, an extension of similar uses is appropriate to capitalize on future investments in infrastructure. Clustering these types of land uses also plays a role in capturing economic synergies between competitive and collaborative firms, cooperative suppliers, and service providers who each share a common need for talent, technology, and infrastructure. A gradient of land uses will transition from heavy industrial to light industrial uses that surround the Cell boundary.

The NCP refines the designation of the subject lands as a combination of Rural Light Industrial and Green Network Study Area. The proposed use of the lands represented by the current subdivision plan is consistent with the direction portrayed in the NCP and with the previous land use approval provided by the RM Council based on the 2016 CDR report.

The NCP defines the basis for locating and designing future regionally significant infrastructure required to support complementary forms of development across multiple jurisdictions.

2.2.1 Transportation

Figure 2.1 illustrates the proposed classification of roadways within the immediate vicinity of the subject lands. According to the NCP, Lutheran Road is intended to eventually transition from a rural connector to a major arterial road. As a major arterial roadway, Lutheran is planned to include six lanes and provide restricted property access. The NCP forecasts the need to acquire additional right-of-way (ROW) at its intersection with Range Road 3052 to accommodate traffic signals. The NCP classifies Range Road 3052 as a minor which is described as a four-lane street requiring a 36 – 41 m ROW. The NCP recommends the dedication of the required ROW needed for arterial roads at the time of subdivision so that development does not prevent future improvements.

The segment of Range Road 3052 extending along the eastern boundary of the subject lands is constructed within a 30.5 m ROW which includes a previous 5-metre easement provided along the subject lands. The initial phase of subdivision was focused along the full extent of the eastern boundary of the property and offers no opportunity to expand the dedication to meet the minor arterial road standards as defined by the NCP.



Figure 2-1: Future Transportation System

A Traffic Impact Study (TIS) was previously prepared to support the 2016 CDR report offering an assessment of the existing municipal road network to accommodate the forecasted traffic resulting from development within the subject lands. The TIS recommended That no traffic improvements related to the proposed development at the intersection of range Rd 3052 and Lutheran road are required except for the installation of yield signs at the intersection of range Rd 3052 and the internal subdivision Road.

AE completed a review of the TIS relative to the proposed subdivision plan and concluded that the alteration of the subdivision plan as presented does not trigger the need to update the original report or its recommendations. A copy of this correspondence is attached to this submission.

2.2.2 Water Distribution

Figure 2-2 illustrates the long-term plan for new regional water distribution within the NCP area. The future water servicing plan identifies the intention to extend a 900-1500 mm water main along the western boundary of the subject lands. This waterline is anticipated to require a 10-metre-wide ROW dedication.

In conjunction with the initial phase of subdivision, a connection was made to the existing SaskWater mainline extending along the eastern boundary of the subject lands. A water pump station was also constructed within the limits of development to provide low-pressure water service to each lot. A permit to construct and operate this system has been previously provided by WSA.



Figure 2-2: Future Water Distribution

The dedication and construction of a 30-metre-wide perimeter drainage swale within the property prohibits the further dedication of any land within the subdivision to account for the proposed future water mainline represented by the NCP. This swale may offer a suitable route for the eventual extension of this planned water main.

2.2.3 Sanitary Sewer

Figure 2-3 illustrates the intention of constructing a 375 – 450 mm sanitary sewer trunk along the northern boundary of the subject lands. The NCP states that future sanitary sewer trunks will require a 20-metre wide ROW dedication and that this dedication should be provided in advance of development to prevent development that would restrict the ability to construct this infrastructure.

The Premium Industrial Park has been designed to utilize septic holding tanks to manage the collection and disposition of wastewater. The dedication and construction of a 30-metre-wide perimeter drainage swale within the property prohibits the further dedication of any land within the subdivision to account for the proposed future sanitary trunk within the bounds of the subject lands. This swale may offer a suitable route for the eventual extension of this planned sanitary trunk.

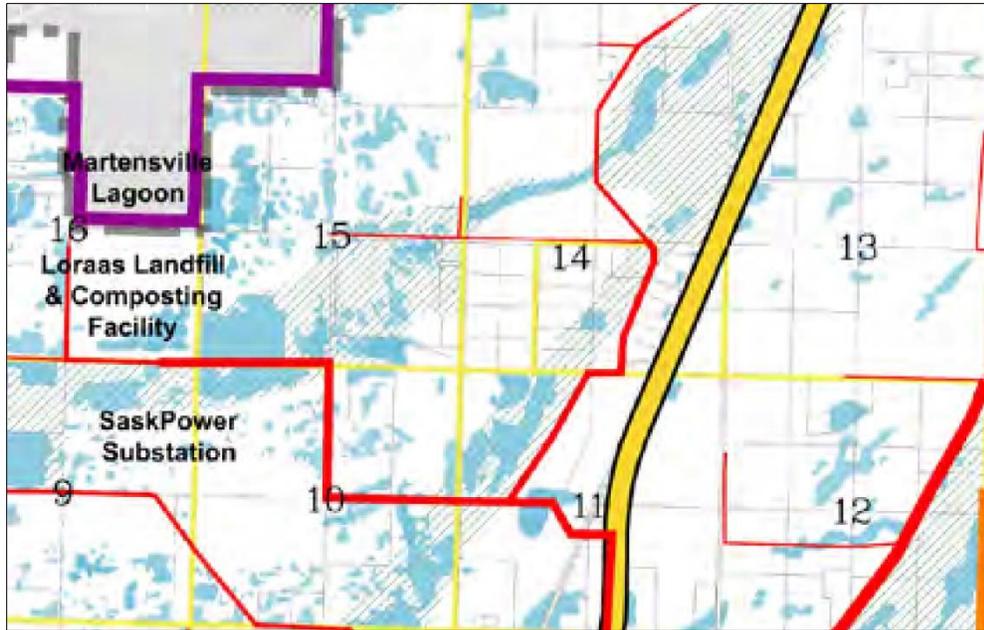


Figure 2-3: Future Sanitary Sewer

2.2.4 Storm Water System

Figure 2-4 illustrates the long-term plan for stormwater management within the NCP area as it relates to the subject lands. The NCP identifies the intention to construct a 1650-1800 mm storm sewer trunk along the north and west boundaries of the ¼ section. The NCP further states that future storm trunk sewers will require a 20-metre-wide ROW dedication and that this dedication should be provided in conjunction with subdivision applications to secure the lands needed for this future infrastructure.

A stormwater management system for the subject lands was dedicated in conjunction with the approval of the first phase of the subdivision and is substantially constructed. The 30-metre-wide drainage swale may offer a suitable option to accommodate future underground storm trunk sewers without impacting the developability of adjacent lands.

2.2.5 Grading

Figure 2-5 illustrates the high-level grading plan for the subject lands as represented in the NCP. The NCP requires the submission of a detailed area grading plan before development that demonstrates that the development drains internally to the desired location and that off-site discharge is managed to not impact the land outside of the plan area.

Detailed construction drawings have been prepared for the subject lands which illustrate how lots are intended to be graded and the routing for stormwater to constructed retention facilities. These drawings have been reviewed by the WSA and deemed acceptable in managing both the predevelopment storage requirements and the incremental run-off generated by hard surface development within the subject lands.

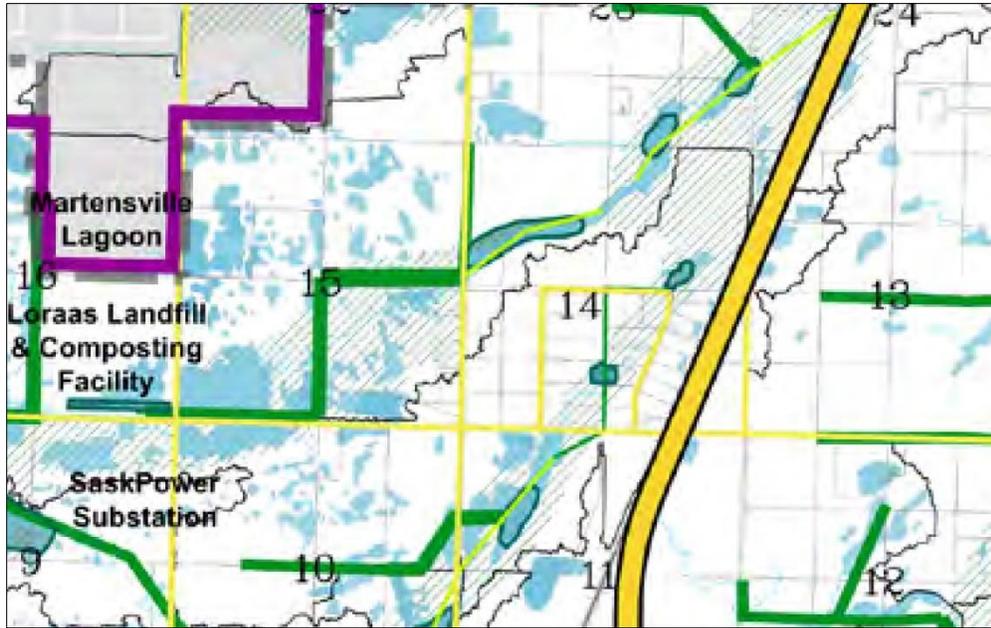


Figure 2-4: Future Storm Water Management

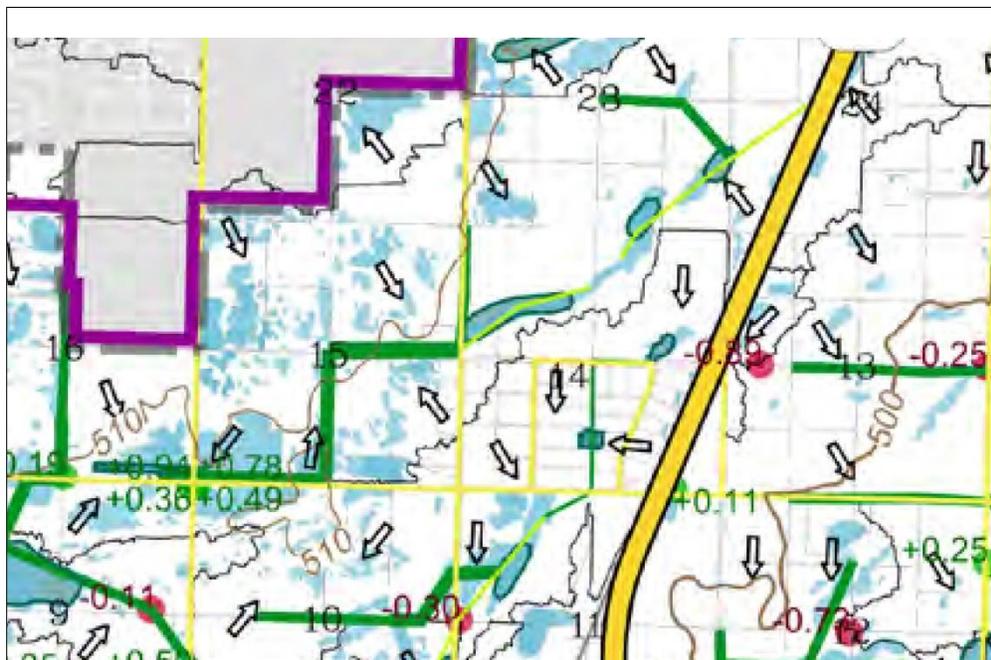


Figure 2-5: Future Grading Plan

2.3 P4G ZONING BYLAW

The subject lands were zoned to a Heavy Industrial District in conjunction with the Council endorsement of the original CDR report. Through the adoption of the subsequent P4G Zoning Bylaw, the subject lands were rezoned to a Light Industrial District. At the time of the original approval, a holding provision was applied to lands lying outside of the initial phase of the subdivision. The following section provides

pertinent excerpts from the P4G Zoning Bylaw and a brief explanation of how the plan of subdivision complies with these regulations.

3.10 Drainage

3.10.1 Where development may alter site drainage potentially affecting adjacent, upstream or downstream properties, or the stability of the land, the applicant shall be required to construct engineered drainage works incorporating sufficient capacity to accommodate the surficial water runoff for a 1:100 year storm event with no incremental increase in offsite flows in excess of what would have been generated from the property prior to the grading and levelling.

Detailed construction drawings were prepared for the subject lands demonstrating the owner's intentions for grading and levelling the sites. A copy of these drawings and a brief memo were circulated to the Water Security Agency for review and to seek confirmation that the change in lot layout will have no impact on the volume of stormwater storage required to support development. As previously noted, the stormwater ponds and perimeter swale were previously dedicated in conjunction with the initial phase of subdivision and the construction of these facilities is substantially complete. A copy of the drawings and memo noted herein has been previously provided to the RM.

3.22. Public Roadways

3.22.1 All public roadways shall be constructed in accordance with the engineering standards of the Municipality or other jurisdiction having authority.

As this application represents a continuation of an existing approved development which was approved based upon the internal roadway being constructed to the standard of the day, the internal roads are designed as gravel-surfaced roads. The owner has indicated that any conversion to a paved road standard in the future would be done as a local improvement.

3.22.2 All development shall have frontage onto and direct physical and legal access to a maintained public roadway, except for:

- a) agricultural operations;*
- b) development internal to a condominium plan containing private roadways; and*
- c) development internal to a dwelling group or multi-use development containing internal roadways as approved by the Development Officer.*

The subdivision application proposes the dedication of a new internal subdivision road which will provide legal and physical access to the proposed new industrial sites.

3.22.6 A traffic impact assessment may be required where the existing traffic counts and patterns are expected to be significantly altered as a result of the proposal.

A TIS report was previously prepared to support the property zoning and initial phase of the subdivision. Following a review of this report, it is our professional opinion that the current report and its recommendations remain valid for the current application.

6.13 D-Light Industrial 1 District

6.13.5 Site Development Regulations

- a) *Site Area - The minimum site area shall be 0.8 ha (2 ac).*
- b) *Site Frontage - The minimum site frontage shall be 30 m (98.4 ft).*

All proposed sites exceed the minimum 0.8 ha lot size and meet the 30-metre frontage requirement.

3 CLOSURE

This report was prepared to describe the changes made to the property layout relative to the concept plan published in the 2016 Comprehensive Development Review report and to offer evidence that the revised subdivision plan submitted aligns with the P4G Official Community Plan and Zoning Bylaw and the North Concept Plan.

Respectfully Submitted,

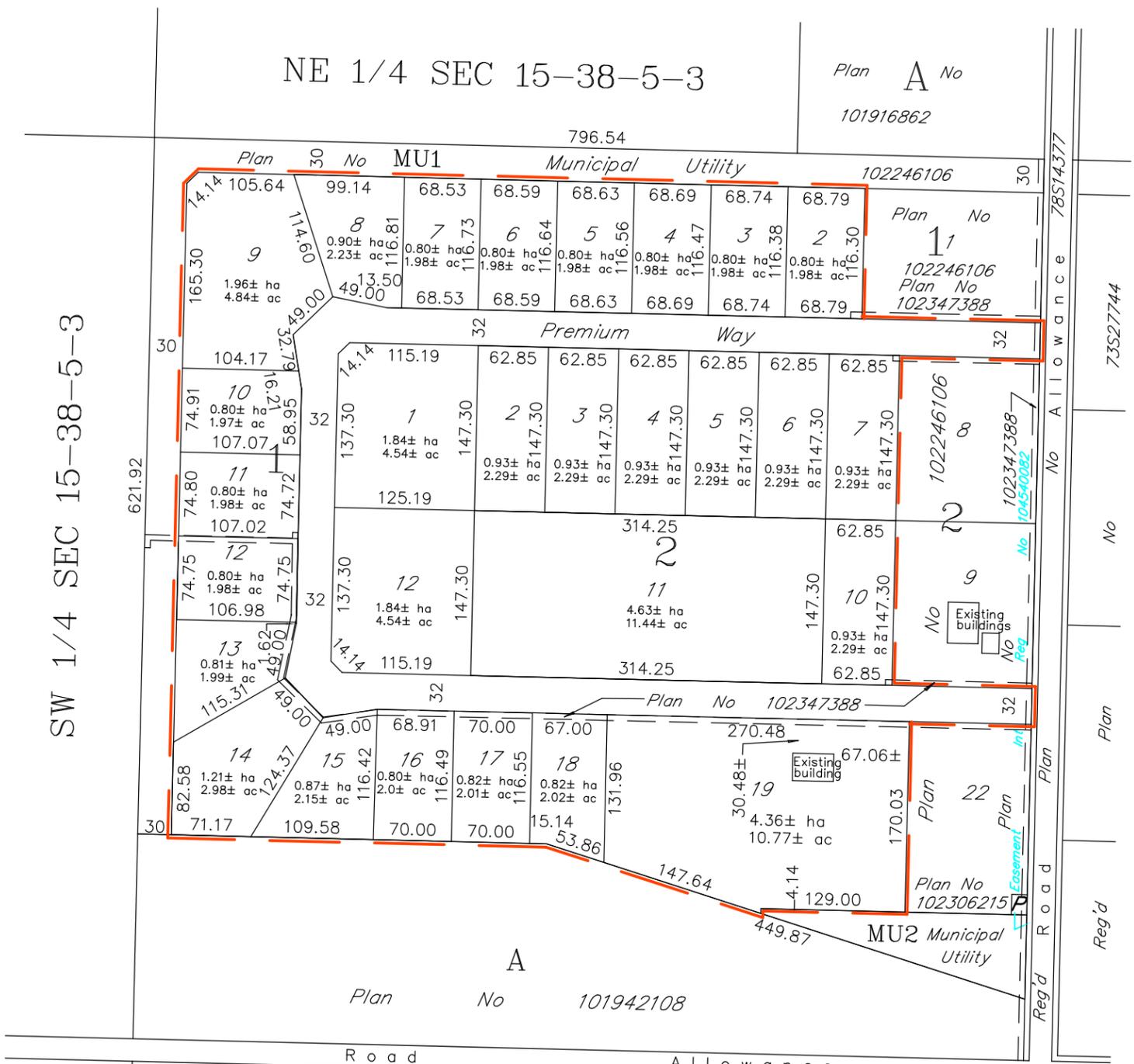
Bill Delainey, RPP MCIP
Project Manager
Associated Engineering Ltd.

Attachments: Plan of Proposed Subdivision (July 25, 2022)
Confirmation Letter – P. Machibroda Engineering (Preliminary Geotechnical Report)
Confirmation Email – Associated Engineering Ltd (TIS Report)

SW 1/4 SEC 15-38-5-3

NE 1/4 SEC 15-38-5-3

Plan A No
101916862



Plan No A 101648167	Plan B	No C 101900641
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OWNER

Registered Owner dd/mm/yy
The signature above indicates that I (we) approve the Plan of Proposed Subdivision as presented.

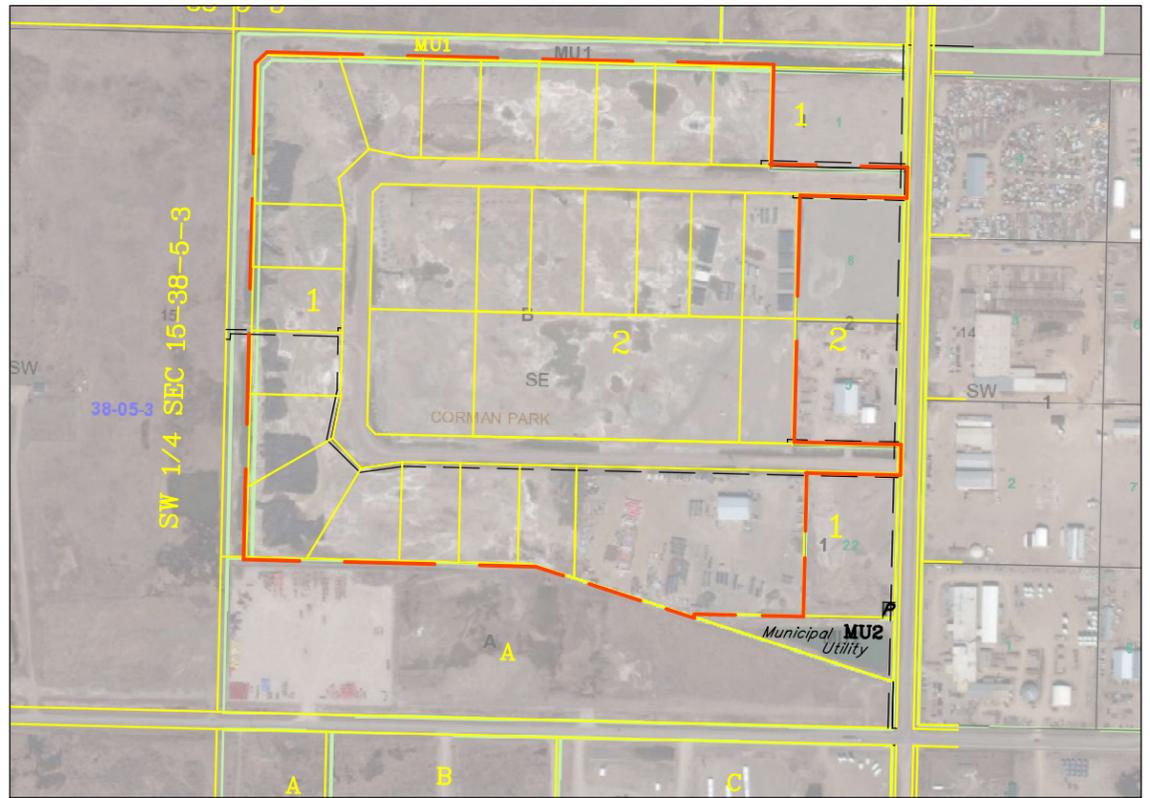
COMMUNITY PLANNING

PLAN SHOWING

**Premium Industrial Park
PLAN OF PROPOSED SUBDIVISION
OF ALL OF PARCEL B , PLAN NO. 102246106
SE1/4 SEC 15, TWP 38, RGE 5, W3 Mer
R.M. OF CORMAN PARK No. 344
SASKATCHEWAN
2022**

SCALE = 1: 5000

AERIAL VIEW



NOTES

- MEASUREMENTS ARE IN METRES AND DECIMALS THEREOF.
- SOME MEASUREMENTS ARE APPROXIMATE AND MAY DIFFER FROM THE FINAL PLAN OF SURVEY BY AS MUCH AS 5 METRES.
- PORTION TO BE APPROVED IS OUTLINED WITH A BOLD DASHED LINE AND CONTAINS 39.79± ha (98.31± ac) WHICH INCLUDES 5.24± ha (12.95± ac) FOR ROADWAY.

SURVEYORS CERTIFICATION

July 25, 2022

Date

Joel Klashinsky
Saskatchewan Land Surveyor



222 JESSOP AVE
SASKATOON, SK
S7N 1Y4
TEL: 306-955-5330

DATE	REVISION / ISSUED	JOB No.	PAGENO
25/07/2022		SC-0087-22cp	
SURVEYED BY:		CALC'D BY: CAS	DRAWN BY: CAS
			REVISION

Bill Delainey

From: Monique Beaudry
Sent: September 23, 2022 3:07 PM
To: Bill Delainey
Cc: Ellen McLaughlin
Subject: RE: 2015-4743-00 - Premium Industrial Park - Traffic Impact Study Validation

Hello Bill,

I have examined the G & S Development Traffic Impact Study, July 2011 by Associated Engineering and the current proposed development, and concluded that an update to the TIA is not needed at this time.

The reason for this is as follows:

- The number of trips for the development site are over-stated in the 2011 TIA Report. The Institute of Transportation Engineers (ITE) trip generation rates were used, without adjusting for the rural Saskatchewan context. ITE trip generation rates are intended for use in urban environments with larger population bases and for developments within larger industrial areas. Since that time, the Ministry of Highways has recognized that this significantly over-states trip generation rates for industrial developments in rural areas. We have been adjusting the rates to be in the range of 20% to 30% of the ITE rate.
- The trip generation was based on number of trips per acre, not number of trips per lot. The total number of acres being developed has not changed.
- Even with the higher trip rates, no traffic improvements related to the proposed development were recommended. An updated TIA would continue to recommend that no traffic improvements related to the development are warranted.

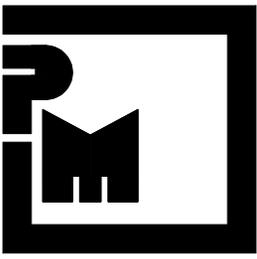
I trust this answers your question. Will you let me know please if you wish to have any clarifications of the above.

Best regards,

Monique Beaudry, P.Eng., RPP, MCIP (she/her/hers)
Senior Transportation Engineer
Associated Engineering (B.C.) Ltd.
#500 - 2889 East 12th Avenue, Vancouver, BC V5M 4T5
Cel: 604.803.2567 | Dir: 236.317.2325
Reception: 604.293.1411



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806-48th STREET EAST
SASKATOON, SK
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F: 306.652.2092

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September 26, 2022

Associated Engineering (Sask.) Ltd.
1-2225 Northridge Drive
Saskatoon, Saskatchewan
S7L 6X6

**ATTENTION: Bill Delaney, RPP MCIP
Manager, Urban Planning**

**RE: RELIANCE LETTER
PRELIMINARY GEOTECHNICAL INVESTIGATION
PROPOSED INDUSTRIAL PARK SUBDIVISION
WITHIN SE 15-38-5 W3M
NEAR MARTENSVILLE, SASKATCHEWAN
PMEL FILE NO. S11-7471.2**

It is understood that P. Machibroda Engineering Ltd. (PMEL) Preliminary Geotechnical Investigation Report No. S11-7471.2 was utilized to support a CDR report submitted for the proposed industrial subdivision in 2016. It is also understood that a new CDR submission for the proposed subdivision includes a total of 32 light industrial lots which exceeds the 20 lots represented in the 2016 CDR. Upon review of the geotechnical report, increasing the lot size from 20 to 32 does not impact the findings of our report. Hence, PMEL Preliminary Geotechnical Report No. S11-7471.2, dated December 8, 2011 may be used to support the 2022 CDR submission for the above referenced project.

Yours very truly,

P. MACHIBRODA ENGINEERING LTD.

Kelly Pardoski, P. Eng.

KP:tbs